



TIMBER NOTICE OF SALE

SALE NAME: LICKETY SPLIT SORTS

AGREEMENT NO: 30-106672 - 30-106678

AUCTION: July 25, 2024 starting at 10:00 a.m.
Pacific Cascade Region Office, Castle Rock, WA

COUNTY: Wahkiakum

SALE LOCATION: Sale located approximately 20 miles west of Longview, WA

**PRODUCTS SOLD
AND SALE AREA:**

All delivered logs, except leave trees marked with blue paint, trees bound with yellow "Leave Tree Area" tags, all down timber existing 5 years prior to the day of sale and all downed timber greater than 60 inches diameter, bound by the following:

Unit 1, white "Timber Sale Boundary" tags with pink flagging and property line marked with white "Timber Sale Boundary" tags, pink flagging and Carsonite posts;

Unit 2, white "Timber Sale Boundary" tags with pink flagging, E-3000 road, E-3005 road and property line marked with white "Timber Sale Boundary" tags, pink flagging and Carsonite posts; meeting the specifications described below; on parts of Section 9 in Township 8 North, Range 5 West W.M., containing 42 acres, more or less.

MINIMUM BID AND ESTIMATED LOG VOLUMES:

| Agreement # | Sort # | Species and Sort Specifications | Average Log Length | Estimated Volume | | Tons Per MBF | Minimum Bid Delivered Prices | | Total Appraised Value | Bid Deposit |
|-------------|--------|---------------------------------|--------------------|------------------|------|--------------|------------------------------|--------|-----------------------|-------------|
| | | | | Mbf | Tons | | \$/mbf | \$/Ton | | |
| 106672 | 1 | DF 5"+ | 28' | 91 | 537 | 5.9 | \$0.00 | | \$0.00 | \$0.00 |
| 106673 | 2 | DF HQ 12"+ | 28' | 65 | 325 | 5 | \$0.00 | | \$0.00 | \$0.00 |
| 106674 | 3 | WW 5"-11" | 28' | 440 | 3080 | 7 | \$0.00 | | \$0.00 | \$0.00 |
| 106675 | 4 | WW 12"+ | 28' | 1735 | 9022 | 5.2 | \$0.00 | | \$0.00 | \$0.00 |
| 106676 | 5 | RC 5"+ | 26' | 24 | 149 | 6.2 | \$0.00 | | \$0.00 | \$0.00 |
| 106677 | 6 | RA 5"+ | 24' | 8 | 56 | 7 | \$0.00 | | \$0.00 | \$0.00 |
| 106678 | 7 | Pulp 2"+ | N/A | 12 | 156 | 13 | | \$0.00 | \$0.00 | \$0.00 |

Totals: **2375 13325 \$0.00**

CERTIFICATION: This sale is certified under the Sustainable Forestry Initiative® program Standard (cert no: BVC-SFIFM-018227)

BID METHOD: Sealed Bids **UNIT OF MEASURE:** MBF Scale/Tonnage Scale

EXPIRATION DATE: November 30, 2024 **ALLOCATION:** Export Restricted

PAYMENT SECURITY: To be determined by the State as described in Clause P-045.2 of the Purchaser's Contract.

BIDDING PROCEDURES: A separate sealed bid and envelope must be submitted for each log sort. Prospective Purchasers may bid on any or all log sorts. On the day of sale the Purchaser must bring their bid deposit up to 10% of their total bid price. Complete bidding procedures and



WASHINGTON STATE DEPARTMENT OF
NATURAL RESOURCES

TIMBER NOTICE OF SALE

auction information may be obtained from the Pacific Cascade Region Office in Castle Rock WA. Phone number (360)577-2025.

TIMBER EXCISE TAX:

Purchaser must pay the forest excise taxes associated with the log sorts delivered to them. The tax rate for this sale is 4.2 %. Taxable Stumpage = Total Delivered Value – (Harvest Cost + Estimated Haul Cost + ARRF). For more information contact the Department of Revenue, Forest Tax Section at 1-800-548-8829.

Use the following rates for estimating taxable stumpage:

Harvest Cost = \$0.00 per MBF for sorts 1, 2, 3, 4, 5 and 6 and \$14.00 per Ton for sort 7.

Hauling Services Payment Rate per Ton
= (Base Rate + Mileage Rate) x (Contractor's hauling bid factor)

Base Rate = \$2.35 per ton

Mileage Rate = ((\$0.16 x C miles) + (\$0.11 x A miles)) x Fuel Index Factor

ARRF = \$0.00 per MBF for sort 7 and \$26.00 per MBF for sorts 1, 2, 3, 4, 5 and 6.

Note: To calculate ARRF rates per ton use the tons/mbf conversion factor in the table above.

Long-haul surcharge: An additional haul payment of \$25/mbf net scale for mbf scale sorts or \$4.60/ton for tonnage sorts will be added for delivery destinations in excess of 250 total one-way miles (A miles plus C miles).

CONFIRMATION: Each sort is subject to confirmation following auction. Sorts will not be confirmed until at least 10 days after auction. Final contract award is contingent upon the State's haul cost analysis. Actual haul route may vary and is subject to change at the State's discretion.

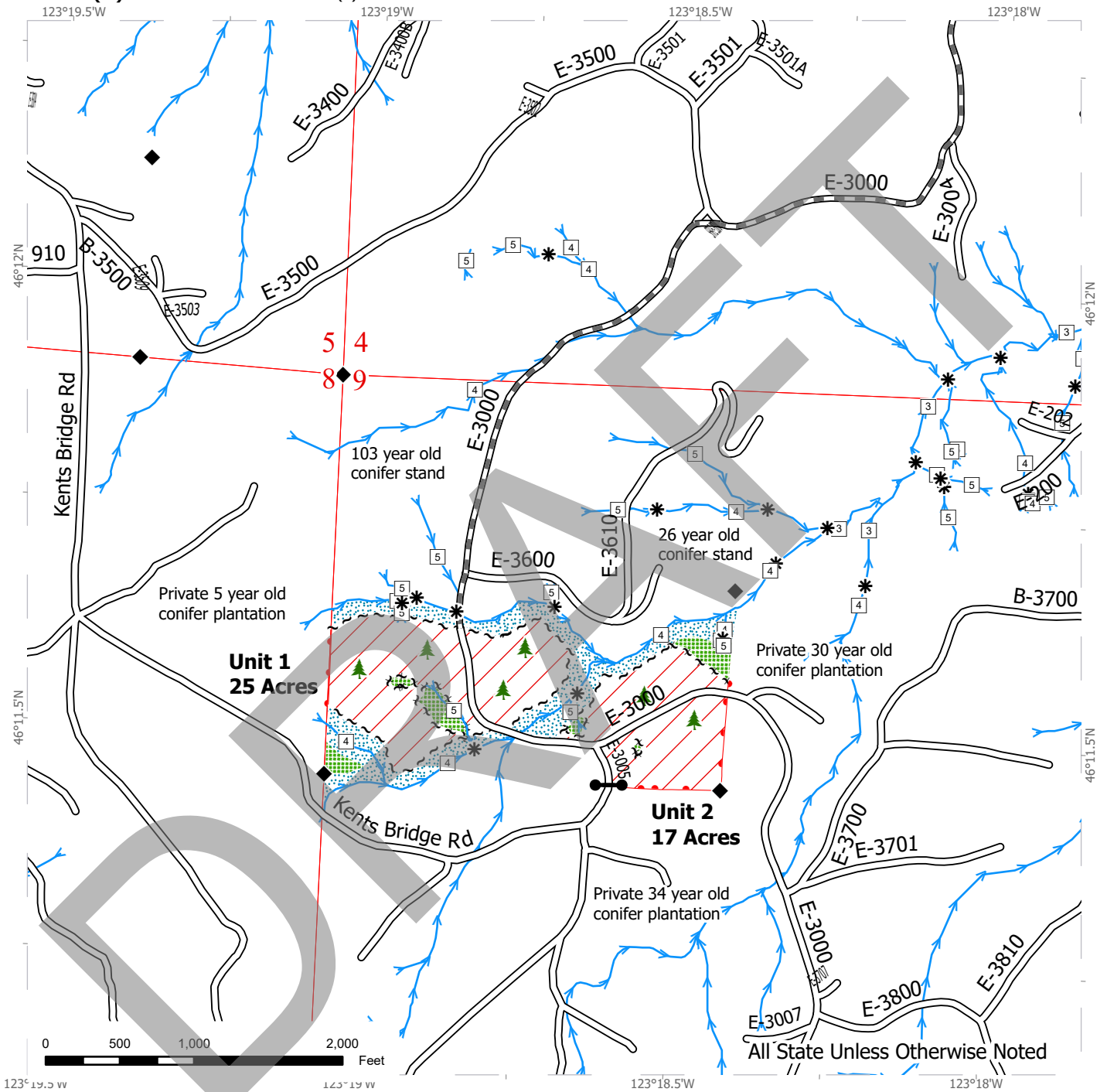
SPECIAL REMARKS: The successful Purchaser(s) will be required to purchase logs from the sale area upon delivery to their location specified in the bid submitted. Logs will be delivered to the Purchaser's delivery location by the State's contract harvester. Purchaser is responsible for weighing and scaling costs. All tonnage loads will be weighed and all mbf loads will be scaled at State approved locations. The State reserves the right to determine where logs are authorized to be scaled and weighed.

For more information regarding this log sort sale visit our web site: <http://www.dnr.wa.gov/programs-and-services/product-sales-and-leasing/timber-sales/timber-auction-packets>. If you have questions call Jon Olson at the Pacific Cascade Region Office at (360)577-2025 or Steve Teitzel at the Product Sales and Leasing Division Office in Olympia at (360)902-1741.

TIMBER SALE MAP

SALE NAME: LICKETY SPLIT SORTS
AGREEMENT #: 30-106665
TOWNSHIP(S): T8R5W
TRUST(S): State Forest Transfer (1)

REGION: Pacific Cascade Region
COUNTY(S): Wahkiakum
ELEVATION RGE: 1240-1400



All State Unless Otherwise Noted

| | | |
|----------------------------|-------------------------------|---------------------------|
| Variable Retention Harvest | Sale Boundary Tags | Streams |
| Leave Tree Area | Leave Tree Tags | Stream Type |
| Riparian Mgt Zone | Property Line | Stream Break |
| Existing Roads | Required Pre-Haul Maintenance | Leave Tree Area <1/4-acre |
| | | Gate (PCP 1-1) |
| | | Survey Monument |



DRIVING MAP

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TRUST(S): State Forest Transfer (1)

REGION: Pacific Cascade Region
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- Harvest Unit
- Highway
- Haul Route
- Other Route
- Milepost Marker
- Distance Indicator
- Gate (PCP 1-1)
- Town (Corporate)

DRIVING DIRECTIONS:

From SR-4 (milepost 35), turn north on Elochoman Valley Road and continue for 3.6 miles. Turn right onto Beaver Creek Road. Stay on Beaver Creek Road for 2.5 miles and turn right onto the E-3000. Continue on the E-3000 for 4.3 miles. Unit 1 will be on both sides of the road. Continue on the E-3000 for .2 miles and Unit 2 will be on both sides of the road.



Timber Sale Cruise Report Lickety Split Sorts

Sale Name: LICKETY SPLIT SORTS

Sale Type: SORT

Region: PACIFIC CASC

District: ST.HELENS

Lead Cruiser: Blake Warnstadt

Other Cruisers:

Cruise Narrative:

Location: Lickety Split is located approximately 4 miles east of Cathlamet. It can be accessed by taking the Elochoman Valley RD to Beaver Creek RD and on to the E-3000. It is located 4.3 miles up the E-3000.

Cruise Design: Unit 1 was cruised using a 46.94 BAF sighted at 4.5 feet. Plots were cruised with a measure to count plot ratio of 1:1. Unit 2 was cruised using a 54.44 BAF sighted at 4.5 feet with a measure to count plot ratio of 2:1. Conifers were cruised to 40 foot preferred log lengths and hardwoods were cruised to 30 foot preferred log lengths. Boles were cruised to 40% of the diameter at 16 feet or 5".

Timber Quality: Both stands in this sale are WH dominant with a small amount of DF, SF, RC and RA. They also both contain an estimated acre of younger timber consisting of WH and DF. There is a fairly high stem count here with trees averaging 9" diameter at 4.5 feet. The majority of this sale the WH has an average DBH of over 20" with good form and is pretty clean. The most common defect observed was spike knots and a small amount of old bear damage. DF on this sale has a 23.9" average DBH with a mix of regular domestic sorts, high quality B, SM and 3P. The SF here is mostly large and clean with a 35.4" average DBH. The RA has an average diameter of 13.9" and looks good. The RC on this sale is mostly in Unit 1. RC has an average diameter of 32" and is pretty clean and solid. Both stands have a couple small openings and some old storm damage. I observed no root disease and minimal insect damage. There was a conk growing in old bear damage on a live WH in unit 1.

Logging and Stand Conditions: This sale is gently sloped and will be harvested using 100% ground based logging methods.

General Remarks:

Timber Sale Notice Volume (MBF)

| Sp | DBH | Rings/In | Age | MBF Volume by Grade | | | | | | |
|-----|------|----------|-----|---------------------|--------|-----------|-------|-------|-------|---------|
| | | | | All | Peeler | Spec Mill | 2 Saw | 3 Saw | 4 Saw | Utility |
| WH | 19.6 | | | 2,164 | | | 1,701 | 369 | 81 | 12 |
| DF | 23.9 | 7.6 | | 156 | 11 | 23 | 106 | 10 | 6 | |
| RC | 32.0 | | | 24 | | | | 24 | | |
| SF | 35.4 | | | 23 | | | 21 | 1 | | |
| RA | 13.9 | | | 8 | | | 4 | | 4 | |
| ALL | 20.0 | 7.6 | | 2,375 | 11 | 23 | 1,832 | 405 | 91 | 12 |

Timber Sale Notice Weight (tons)

| Sp | Tons by Grade | | | | | | |
|-----|---------------|--------|-----------|--------|-------|-------|---------|
| | All | Peeler | Spec Mill | 2 Saw | 3 Saw | 4 Saw | Utility |
| WH | 16,313 | | | 11,814 | 3,522 | 854 | 124 |
| DF | 1,032 | 57 | 176 | 662 | 95 | 42 | |
| RC | 168 | | | | 168 | | |
| SF | 131 | | | 118 | 13 | | |
| RA | 74 | | | 28 | | 46 | |
| ALL | 17,719 | 57 | 176 | 12,622 | 3,798 | 942 | 124 |

Timber Sale Overall Cruise Statistics

| BA (sq ft/acre) | BA SE (%) | V-BAR (bf/sq ft) | V-BAR SE (%) | Net Vol (bf/acre) | Vol SE (%) |
|--------------------|--------------|---------------------|-----------------|----------------------|---------------|
| 279.5 | 4.0 | 202.2 | 2.4 | 56,540 | 4.7 |

Timber Sale Unit Cruise Design

| Unit | Design | Cruise Acres | FMA Acres | N Plots | N Cruise Plots | N Void Plots |
|------------------------|--|--------------|-----------|---------|----------------|--------------|
| LICKETY SPLIT SORTS U1 | B1C: VR, 1 BAF (46.94) Measure/Count Plots, Sighting Ht = 4.5 ft | 25.0 | 26.8 | 27 | 15 | 0 |
| LICKETY SPLIT SORTS U2 | B1C: VR, 1 BAF (54.44) Measure/Count Plots, Sighting Ht = 4.5 ft | 17.0 | 15.9 | 14 | 9 | 0 |
| All | | 42.0 | 42.7 | 41 | 24 | 0 |

Timber Sale Log Grade x Sort Summary

| Sp | Status | Grade | Sort | Dia | Len | BF Gross | BF Net | Defect % | Tons | MBF Net |
|----|--------|--------------|----------|------|-----|----------|--------|----------|----------|---------|
| DF | LIVE | 2 SAW | Domestic | 17.4 | 40 | 1,650 | 1,514 | 8.2 | 401.8 | 63.6 |
| DF | LIVE | 2 SAW | HQ-B | 18.7 | 40 | 1,061 | 1,002 | 5.5 | 260.1 | 42.1 |
| DF | LIVE | 3 PEELER | Domestic | 30.1 | 40 | 269 | 269 | 0.0 | 57.4 | 11.3 |
| DF | LIVE | 3 SAW | Domestic | 9.6 | 32 | 248 | 239 | 3.9 | 94.9 | 10.0 |
| DF | LIVE | 4 SAW | Domestic | 5.2 | 35 | 148 | 141 | 4.7 | 41.9 | 5.9 |
| DF | LIVE | SPECIAL MILL | HQ-A | 19.1 | 40 | 554 | 554 | 0.0 | 176.2 | 23.3 |
| RA | LIVE | 2 SAW | Domestic | 12.2 | 30 | 97 | 83 | 13.6 | 28.1 | 3.5 |
| RA | LIVE | 4 SAW | Domestic | 6.5 | 26 | 122 | 105 | 14.0 | 46.4 | 4.4 |
| RC | LIVE | 3 SAW | Domestic | 14.8 | 40 | 601 | 568 | 5.6 | 167.5 | 23.8 |
| SF | LIVE | 2 SAW | Domestic | 22.2 | 40 | 528 | 507 | 4.1 | 118.3 | 21.3 |
| SF | LIVE | 3 SAW | Domestic | 11.6 | 27 | 37 | 35 | 3.8 | 13.1 | 1.5 |
| WH | LIVE | 2 SAW | Domestic | 16.4 | 40 | 42,075 | 40,506 | 3.7 | 11,813.6 | 1,701.2 |

| Sp | Status | Grade | Sort | Dia | Len | BF Gross | BF Net | Defect % | Tons | MBF Net |
|----|--------|---------|----------|-----|-----|----------|--------|----------|---------|---------|
| WH | LIVE | 3 SAW | Domestic | 9.2 | 37 | 9,246 | 8,794 | 4.9 | 3,522.2 | 369.4 |
| WH | LIVE | 4 SAW | Domestic | 6.0 | 27 | 2,016 | 1,928 | 4.4 | 853.7 | 81.0 |
| WH | LIVE | CULL | Cull | 5.9 | 4 | 119 | 0 | 100.0 | 0.0 | 0.0 |
| WH | LIVE | UTILITY | Pulp | 6.9 | 13 | 296 | 294 | 0.6 | 123.8 | 12.4 |

Timber Sale Log Sort x Diameter Bin Summary

| Sp | Bin | Status | Sort | Dia | Len | BF Net | Defect % | Tons | MBF Net |
|----|---------|--------|----------|------|-----|--------|----------|---------|---------|
| DF | 5 - 7 | LIVE | Domestic | 5.2 | 34 | 141 | 4.7 | 41.9 | 5.9 |
| DF | 8 - 11 | LIVE | Domestic | 9.6 | 33 | 239 | 3.9 | 94.9 | 10.0 |
| DF | 12 - 15 | LIVE | HQ-A | 12.2 | 40 | 84 | 0.0 | 63.9 | 3.5 |
| DF | 12 - 15 | LIVE | HQ-B | 12.2 | 40 | 100 | 2.6 | 34.0 | 4.2 |
| DF | 12 - 15 | LIVE | Domestic | 14.9 | 40 | 361 | 5.0 | 103.6 | 15.2 |
| DF | 16 - 19 | LIVE | Domestic | 16.6 | 40 | 513 | 5.4 | 135.4 | 21.5 |
| DF | 20+ | LIVE | HQ-A | 21.6 | 40 | 470 | 0.0 | 112.3 | 19.8 |
| DF | 20+ | LIVE | HQ-B | 22.7 | 40 | 902 | 5.8 | 226.2 | 37.9 |
| DF | 20+ | LIVE | Domestic | 22.8 | 40 | 909 | 8.7 | 220.2 | 38.2 |
| RA | 5 - 7 | LIVE | Domestic | 5.2 | 23 | 42 | 13.5 | 19.0 | 1.8 |
| RA | 8 - 11 | LIVE | Domestic | 8.4 | 30 | 63 | 14.3 | 27.4 | 2.7 |
| RA | 12 - 15 | LIVE | Domestic | 12.2 | 30 | 83 | 13.6 | 28.1 | 3.5 |
| RC | 5 - 7 | LIVE | Domestic | 7.5 | 40 | 26 | 0.0 | 11.7 | 1.1 |
| RC | 8 - 11 | LIVE | Domestic | 11.0 | 39 | 43 | 7.7 | 19.5 | 1.8 |
| RC | 12 - 15 | LIVE | Domestic | 15.2 | 40 | 136 | 9.3 | 40.8 | 5.7 |
| RC | 20+ | LIVE | Domestic | 24.1 | 40 | 363 | 4.2 | 95.6 | 15.2 |
| SF | 8 - 11 | LIVE | Domestic | 11.1 | 22 | 15 | 0.0 | 4.9 | 0.6 |
| SF | 12 - 15 | LIVE | Domestic | 12.2 | 32 | 20 | 6.4 | 8.2 | 0.9 |
| SF | 16 - 19 | LIVE | Domestic | 17.0 | 40 | 72 | 5.0 | 19.2 | 3.0 |
| SF | 20+ | LIVE | Domestic | 24.1 | 40 | 435 | 4.0 | 99.1 | 18.3 |
| WH | 5 - 7 | LIVE | Cull | 5.6 | 4 | 0 | 100.0 | 0.0 | 0.0 |
| WH | 5 - 7 | LIVE | Domestic | 6.2 | 31 | 3,340 | 3.2 | 1,443.3 | 140.3 |
| WH | 5 - 7 | LIVE | Pulp | 6.2 | 13 | 187 | 0.0 | 71.0 | 7.8 |
| WH | 8 - 11 | LIVE | Pulp | 8.8 | 14 | 108 | 1.5 | 52.9 | 4.5 |
| WH | 8 - 11 | LIVE | Cull | 8.9 | 3 | 0 | 100.0 | 0.0 | 0.0 |
| WH | 8 - 11 | LIVE | Domestic | 9.8 | 35 | 7,114 | 5.4 | 2,828.4 | 298.8 |
| WH | 12 - 15 | LIVE | Domestic | 13.8 | 40 | 13,101 | 3.5 | 4,258.0 | 550.3 |
| WH | 16 - 19 | LIVE | Domestic | 17.8 | 40 | 17,237 | 3.3 | 4,876.2 | 724.0 |
| WH | 20+ | LIVE | Domestic | 22.4 | 40 | 10,435 | 4.9 | 2,783.6 | 438.3 |

Cruise Unit Report LICKETY SPLIT SORTS U1

Unit Sale Notice Volume (MBF): LICKETY SPLIT SORTS U1

| Sp | DBH | Rings/In | Age | MBF Volume by Grade | | | | | |
|-----|------|----------|-----|---------------------|-----------|-------|-------|-------|---------|
| | | | | All | Spec Mill | 2 Saw | 3 Saw | 4 Saw | Utility |
| WH | 19.2 | | | 1,191 | | 912 | 220 | 53 | 5 |
| DF | 20.0 | 8.0 | | 101 | 20 | 70 | 6 | 5 | |
| RC | 32.0 | | | 24 | | | 24 | | |
| SF | 35.4 | | | 23 | | 21 | 1 | | |
| RA | 13.9 | | | 8 | | 4 | | 4 | |
| ALL | 19.5 | 8.0 | | 1,346 | 20 | 1,007 | 251 | 63 | 5 |

Unit Cruise Design: LICKETY SPLIT SORTS U1

| Design | Cruise Acres | FMA Acres | N Plots | N Cruise Plots | N Void Plots |
|--|--------------|-----------|---------|----------------|--------------|
| B1C: VR, 1 BAF (46.94) Measure/Count Plots, Sighting Ht = 4.5 ft | 25.0 | 26.8 | 27 | 15 | 0 |

Unit Cruise Summary: LICKETY SPLIT SORTS U1

| Sp | Cruised Trees | All Trees | Trees/Plot | Ring-Count Trees |
|-----|---------------|-----------|------------|------------------|
| WH | 73 | 137 | 5.1 | 0 |
| DF | 11 | 11 | 0.4 | 2 |
| RC | 4 | 4 | 0.1 | 0 |
| SF | 2 | 2 | 0.1 | 0 |
| RA | 2 | 2 | 0.1 | 0 |
| ALL | 92 | 156 | 5.8 | 2 |

Unit Cruise Statistics: LICKETY SPLIT SORTS U1

| Sp | BA (sq ft/acre) | BA CV (%) | BA SE (%) | V-BAR (bf/sq ft) | V-BAR CV (%) | V-BAR SE (%) | Net Vol (bf/acre) | Vol CV (%) | Vol SE (%) |
|-----|-----------------|-----------|-----------|------------------|--------------|--------------|-------------------|------------|------------|
| WH | 238.2 | 32.3 | 6.2 | 200.0 | 30.0 | 3.5 | 47,640 | 44.1 | 7.1 |
| DF | 19.1 | 183.4 | 35.3 | 211.1 | 24.7 | 7.5 | 4,037 | 185.1 | 36.1 |
| RC | 7.0 | 406.0 | 78.1 | 137.1 | 3.0 | 1.5 | 954 | 406.0 | 78.2 |
| SF | 3.5 | 360.3 | 69.3 | 261.9 | 13.9 | 9.8 | 911 | 360.6 | 70.0 |
| RA | 3.5 | 519.6 | 100.0 | 91.2 | 11.1 | 7.8 | 317 | 519.7 | 100.3 |
| ALL | 271.2 | 24.2 | 4.6 | 198.6 | 30.7 | 3.2 | 53,859 | 39.0 | 5.6 |

Unit Summary: LICKETY SPLIT SORTS U1

| Sp | Status | Rx | N | D | DBH | BL | THT | BF Gross | BF Net | Defect % | TPA | BA | RD | MBF Net |
|-----|--------|-----|----|-----|------|-----|-----|-------------|-----------|-------------|-------|-------|------|------------|
| DF | LIVE | CUT | 11 | ALL | 20.0 | 67 | 100 | 4,280 | 4,037 | 5.7 | 8.8 | 19.1 | 4.3 | 100.9 |
| RA | LIVE | CUT | 2 | ALL | 13.9 | 55 | 71 | 368 | 317 | 13.8 | 3.3 | 3.5 | 0.9 | 7.9 |
| RC | LIVE | CUT | 4 | ALL | 32.0 | 82 | 104 | 1,010 | 954 | 5.6 | 1.2 | 7.0 | 1.2 | 23.8 |
| SF | LIVE | CUT | 2 | ALL | 35.4 | 110 | 141 | 950 | 911 | 4.1 | 0.5 | 3.5 | 0.6 | 22.8 |
| WH | LIVE | CUT | 73 | ALL | 18.1 | 74 | 97 | 49,724 | 47,640 | 4.2 | 133.3 | 238.2 | 56.0 | 1,191.0 |
| ALL | LIVE | CUT | 92 | ALL | 18.4 | 73 | 97 | 56,331 | 53,859 | 4.4 | 147.1 | 271.2 | 63.0 | 1,346.5 |
| ALL | ALL | ALL | 92 | ALL | 18.4 | 73 | 97 | 56,331 | 53,859 | 4.4 | 147.1 | 271.2 | 63.0 | 1,346.5 |

DRAFT

Cruise Unit Report LICKETY SPLIT SORTS U2

Unit Sale Notice Volume (MBF): LICKETY SPLIT SORTS U2

| Sp | DBH | Rings/In | Age | MBF Volume by Grade | | | | | | |
|-----|------|----------|-----|---------------------|--------|-----------|-------|-------|-------|---------|
| | | | | All | Peeler | Spec Mill | 2 Saw | 3 Saw | 4 Saw | Utility |
| WH | 20.1 | | | 973 | | | 789 | 149 | 28 | 7 |
| DF | 31.1 | 7.0 | | 55 | 11 | 4 | 36 | 4 | 1 | |
| ALL | 20.6 | 7.0 | | 1,028 | 11 | 4 | 825 | 153 | 28 | 7 |

Unit Cruise Design: LICKETY SPLIT SORTS U2

| Design | Cruise Acres | FMA Acres | N Plots | N Cruise Plots | N Void Plots |
|--|--------------|-----------|---------|----------------|--------------|
| B1C: VR, 1 BAF (54.44) Measure/Count Plots, Sighting Ht = 4.5 ft | 17.0 | 15.9 | 14 | 9 | 0 |

Unit Cruise Summary: LICKETY SPLIT SORTS U2

| Sp | Cruised Trees | All Trees | Trees/Plot | Ring-Count Trees |
|-----|---------------|-----------|------------|------------------|
| WH | 46 | 71 | 5.1 | 0 |
| DF | 4 | 4 | 0.3 | 1 |
| ALL | 50 | 75 | 5.4 | 1 |

Unit Cruise Statistics: LICKETY SPLIT SORTS U2

| Sp | BA (sq ft/acre) | BA CV (%) | BA SE (%) | V-BAR (bf/sq ft) | V-BAR CV (%) | V-BAR SE (%) | Net Vol (bf/acre) | Vol CV (%) | Vol SE (%) |
|-----|-----------------|-----------|-----------|------------------|--------------|--------------|-------------------|------------|------------|
| WH | 276.1 | 29.4 | 7.9 | 207.3 | 23.3 | 3.4 | 57,231 | 37.5 | 8.6 |
| DF | 15.6 | 164.1 | 43.9 | 209.1 | 41.3 | 20.6 | 3,252 | 169.2 | 48.5 |
| ALL | 291.6 | 27.0 | 7.2 | 207.4 | 24.6 | 3.5 | 60,482 | 36.5 | 8.0 |

Unit Summary: LICKETY SPLIT SORTS U2

| Sp | Status | Rx | N | D | DBH | BL | THT | BF Gross | BF Net | Defect % | TPA | BA | RD | MBF Net |
|-----|--------|-----|----|-----|------|-----|-----|----------|--------|----------|-------|-------|------|---------|
| DF | LIVE | CUT | 4 | ALL | 31.1 | 115 | 148 | 3,413 | 3,252 | 4.7 | 2.9 | 15.6 | 2.8 | 55.3 |
| WH | LIVE | CUT | 46 | ALL | 18.3 | 84 | 109 | 59,675 | 57,231 | 4.1 | 151.2 | 276.1 | 64.5 | 972.9 |
| ALL | LIVE | CUT | 50 | ALL | 18.6 | 85 | 110 | 63,088 | 60,482 | 4.1 | 154.1 | 291.6 | 67.3 | 1,028.2 |
| ALL | ALL | ALL | 50 | ALL | 18.6 | 85 | 110 | 63,088 | 60,482 | 4.1 | 154.1 | 291.6 | 67.3 | 1,028.2 |

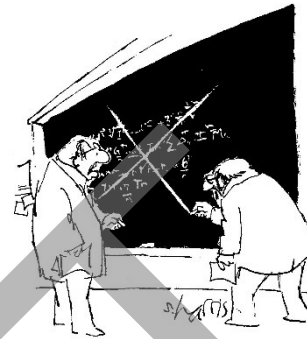
FPHP NEEDED (Y/N) N

Is abandonment of existing road required? (Y/N) N

PACIFIC CASCADE REGION - ENGINEERING

ROAD PLAN PEER REVIEW CHECKLIST

PROJECT: LICKETY SPLIT



This project has been reviewed for the following:

Initials:

JM

CONTRACT CLAUSES – Selection of proper clauses. Clauses adequately describe desired work. Clauses do not conflict with maps, details, pit plans, etc. Punctuation, syntax, grammar and organization is correct.

JM

TYPICAL SECTION SHEET, ROCK LIST, & CULVERT LIST – Sheets match clauses and maps. Requirements and quantities make sense. Rock List adds up correctly.

JM

MAPS – All roads listed in Section 1 are shown on maps. Maps identify locations of all culverts, landings, waste areas, endhaul/overhaul areas, etc. Legend, north arrow and scale are shown. Line types are easy to identify. Map is at a legible scale.

JM

DETAIL SHEETS – All detail sheets referred to in the clauses are included. Detail sheets have been edited as necessary.

PIT PLANS – Selection of proper clauses. Map clearly shows all areas of development, wasting, stockpiling, reclamation, etc. Development plan appears logical for long term use of pit. Development plan allows for safe operation in the pit.

JM

ROAD COST SPREADSHEET – All cost elements captured. Material costs used are current. Summary cells are adding correctly. No conflicts exist between pages. Stationing, culverts and rock volume match the road plan.

JM

EXCISE TAX SHEET – Totals match road plan.

LOGGING PLAN – Plan matches road plan clauses and maps.

I certify that I have reviewed this project for the elements initialed above and have found that it meets or exceeds Department and Regional Standards to the best of my knowledge.

CHACE JOHANSON

Originator of Project

01/19/2024

Date


Peer Reviewer

1/25/2024

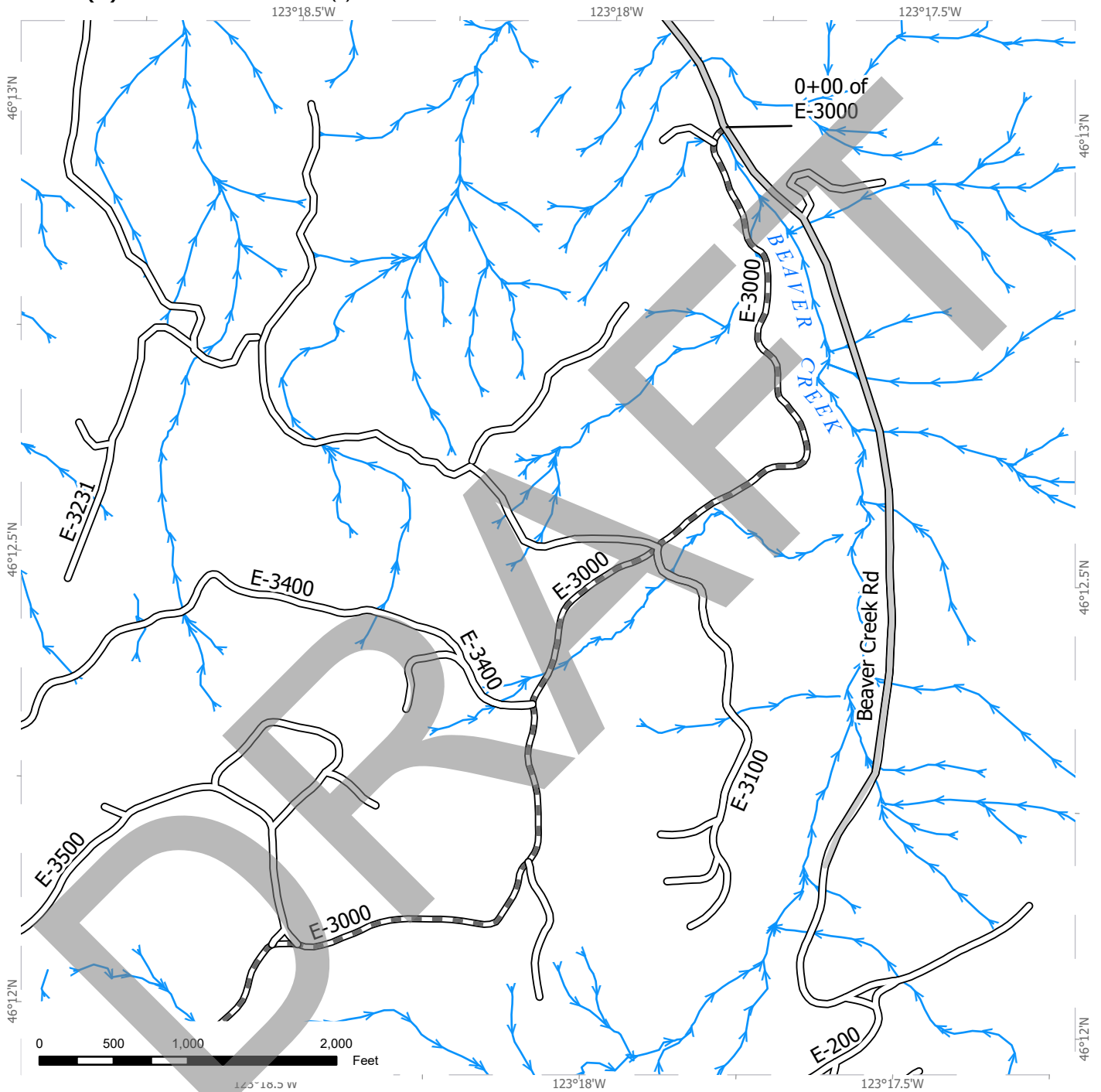
Date





Comments:

ROAD PLAN MAP

SALE NAME: LICKETY SPLIT SORTS
AGREEMENT #: 30-106665
TOWNSHIP(S): T8R5W
TRUST(S): State Forest Transfer (1)

REGION: Pacific Cascade Region
COUNTY(S): Wahkiakum
ELEVATION RGE: 1240-1400



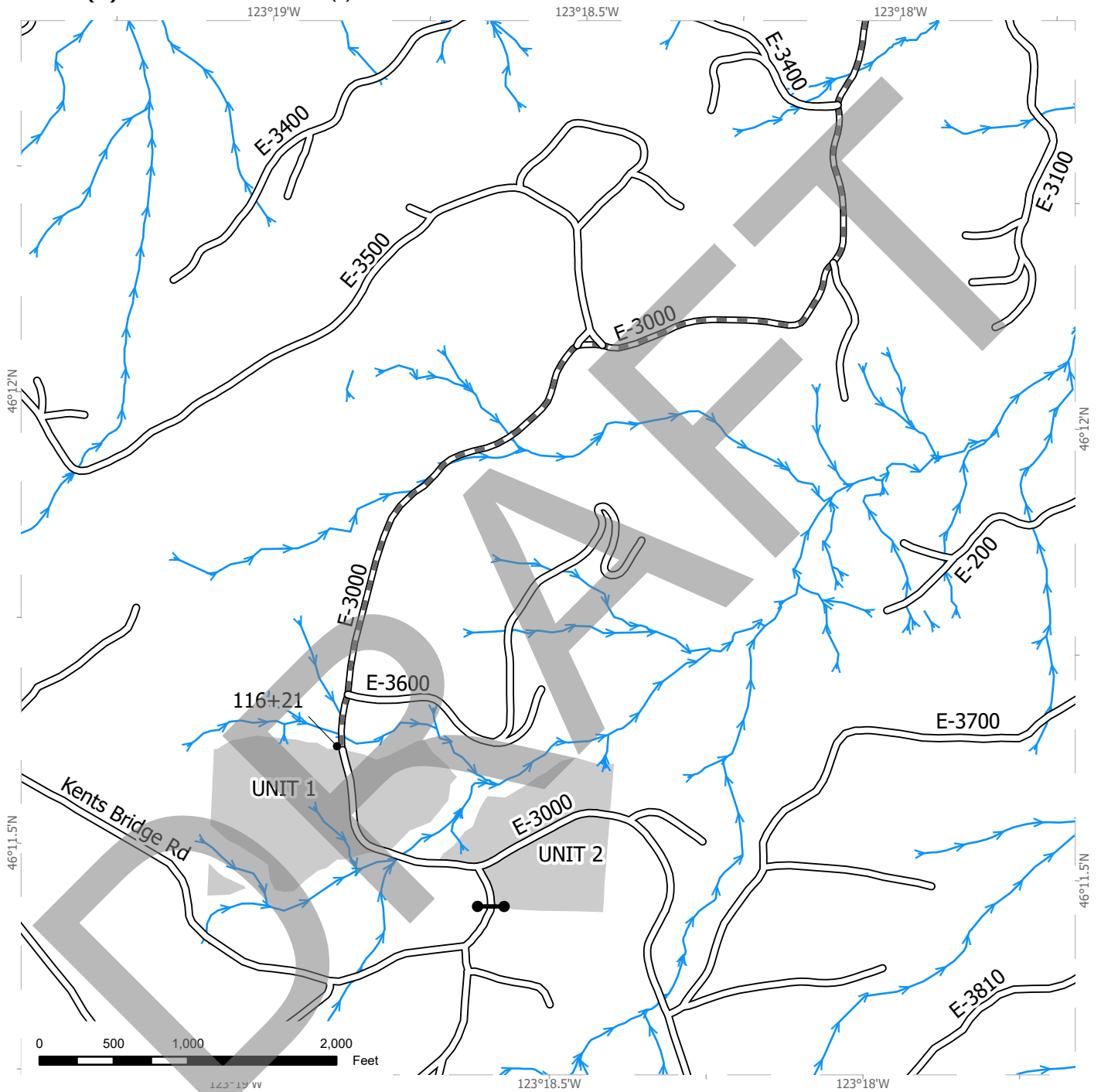
-  County Road
-  Existing Roads
-  Required Pre-Haul Maintenance
-  Streams


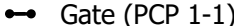





ROAD PLAN MAP

SALE NAME: LICKETY SPLIT SORTS
AGREEMENT #: 30-106665
TOWNSHIP(S): T8R5W
TRUST(S): State Forest Transfer (1)

REGION: Pacific Cascade Region
COUNTY(S): Wahkiakum
ELEVATION RGE: 1240-1400



| | | |
|---|---|--|
|  Existing Roads |  Gate (PCP 1-1) |  Harvest Unit |
|  Required Pre-Haul Maintenance | | |
|  Streams | | |



STATE OF WASHINGTON
DEPARTMENT OF NATURAL RESOURCES

LICKETY SPLIT SORTS TIMBER SALE ROAD PLAN
WAHKIAKUM COUNTY
ST HELENS DISTRICT
PACIFIC CASCADE REGION

AGREEMENT NO.: 30-106665

STAFF ENGINEER: CHACE JOHANSON

DRAWN & COMPILED BY: ALICIA COMPTON

SECTION 0 – SCOPE OF PROJECT

0-1 ROAD PLAN SCOPE

Clauses in this road plan apply to all road related work, including landings and rock source development, unless otherwise noted.

0-2 REQUIRED ROADS

The specified work on the following roads is required.

| <u>Road</u> | <u>Stations</u> | <u>Type</u> |
|-------------|-----------------|-------------|
| E-3000 | 0+00 to 116+21 | Pre-Haul |

0-6 PRE-HAUL MAINTENANCE

This project includes, but is not limited to the following pre-haul maintenance requirements:

| <u>Road</u> | <u>Stations</u> | <u>Requirements</u> |
|-------------|-----------------|---|
| E-3000 | 0+00 to 116+21 | Grade existing road surface, rip potholes, shape to crown in accordance with the TYPICAL SECTION SHEET, compact |

1-1 ROAD PLAN CHANGES

If the Contractor desires a change from this road plan including, but not limited to, relocation, extension, change in design, or adding roads; a revised road plan must be submitted in writing to the Contract Administrator for consideration. Before work begins, Contractor shall obtain approval from the State for the submitted plan.

1-2 NON-COMPLIANCE WITH STATE ROAD PLAN

Quantities established in this road plan are minimum acceptable values. Additional quantities required by the state due to non-compliance or the Contractor's choice of construction techniques will be at the Contractor's expense.

1-3 ROAD DIMENSIONS

Contractor shall perform road work in accordance with the dimensions shown on the TYPICAL SECTION SHEET and the specifications within this road plan.

1-4 ROAD TOLERANCES

Contractor shall perform road work within the tolerances listed below. The tolerance class for each road is listed on the TYPICAL SECTION SHEET.

| <u>Tolerance Class</u> | <u>A</u> | <u>B</u> | <u>C</u> |
|-------------------------------------|----------|----------|----------|
| Road and Subgrade Width (feet) | +1.5 | +1.5 | +2.0 |
| Subgrade Elevation (feet +/-) | 0.5 | 1.0 | 2.0 |
| Centerline alignment (feet lt./rt.) | 1.0 | 1.5 | 3.0 |

1-6 ORDER OF PRECEDENCE

Any conflict or inconsistency in the road plan will be resolved by giving the documents precedence in the following order:

1. Addenda.
2. Designs or Plans. On designs and plans, figured dimensions shall take precedence over scaled dimensions.
3. Road Plan Clauses.
4. Typical Section Sheet.
5. Standard Lists.
6. Standard Details.
7. Road Plan maps.

In case of any ambiguity or dispute over interpreting the road plan, the Contract Administrator’s or designee’s decision will be final.

1-8 REPAIR OR REPLACEMENT OF DAMAGED MATERIALS

Contractor shall repair or replace all materials, roadway infrastructure, and road components damaged during road work or operation activities. The Contract Administrator will direct repairs and replacements. Repairs to structural materials must be made in accordance with the manufacturer’s recommendation and may not begin without written approval from the Contract Administrator.

1-9 DAMAGED METALLIC COATING

Any cut ends, or damaged galvanized or aluminized coating on existing or new bridge components, culverts, downspouts, and flumes must be cleaned and treated with a minimum of two coats of zinc rich paint or cold galvanizing compound.

1-15 ROAD MARKING

Contractor shall perform road work in accordance with the state’s marked location. All road work is marked as follows:

- Orange paint or orange flagging for pre-haul maintenance.

1-21 HAUL APPROVAL

Contractor shall not use roads under this road plan for timber haul, other than timber cut on the right-of-way, without written approval from the Contract Administrator.

1-22 WORK NOTIFICATIONS

Contractor shall notify the Contract Administrator a minimum of 3 business days before work begins.

1-27 TIMING RESTRICTION FOR MARBLED MURRELET

On the following road(s), any road work, right-of-way timber falling and yarding, rock pit operation, or heavy equipment operation is not allowed from one hour before official sunrise to two hours after official sunrise, and from one hour before official sunset to one hour after official sunset from April 1 through September 23. This restriction does not apply to hauling timber, rock, or equipment.

| <u>Road</u> | <u>Stations</u> |
|-------------|-----------------|
| E-3000 | 56+99 to 94+13 |

1-29 SEDIMENT RESTRICTION

Contractor shall not allow silt-bearing runoff to enter any streams.

1-30 CLOSURE TO PREVENT DAMAGE

In accordance with Contract Clause G-220.1 STATE SUSPENDS OPERATION, the Contract Administrator will suspend road work or hauling right-of-way timber, forest products, or rock under the following conditions:

- Wheel track rutting exceeds 6 inches on jaw run pit run roads.
- Wheel track rutting exceeds 4 inches on crushed rock roads.
- Wheel track rutting exceeds 8 inches on native surface roads.
- Surface or base stability problems persist.
- Weather is such that satisfactory results cannot be obtained in an area of operations.
- When, in the opinion of the Contract Administrator, excessive road damage or rutting may occur.

Operations must stop unless authority to continue working or hauling is granted in writing by the Contract Administrator. In the event that surface or base stability problems persist, Contractor shall cease operations, or perform corrective maintenance or repairs, subject to specifications within this road plan. Before and during any suspension, Contractor shall protect the work from damage or deterioration.

1-32 BRIDGE OR ASPHALT SURFACE RESTRICTION

The use of metal tracked equipment is not allowed on bridge or asphalt surfaces at any time. If Contractor must run equipment on bridge or asphalt surfaces, then rubber tired equipment or other methods, approved in writing by Contract Administrator, must be used.

If tracked equipment is used on bridge or asphalt surfaces, Contractor shall immediately cease all road construction and hauling operations. Contractor shall remove any dirt, rock, or other material tracked or spilled on the bridge or asphalt surface(s) and have surface(s) evaluated by the Region Engineer or their designee for any damage caused by transporting equipment. Any damage to the surface(s) will be repaired, at the Contractor's expense, as directed by the Contract Administrator.

1-33 SNOW PLOWING RESTRICTION

Snowplowing will be allowed after the execution of a SNOW PLOWING AGREEMENT, which is available from the Contract Administrator upon request. Contractor shall request a SNOW PLOWING AGREEMENT each time plowing occurs. If damage occurs while plowing, further permission to plow may be revoked by the Contract Administrator.

1-40 ROAD APPROACHES TO COUNTY ROADS AND STATE HIGHWAYS

Contractor shall immediately remove any mud, dirt, rock, or other material tracked or spilled on to county roads and state highways.

If additional damage to the surface, signs, guardrails, etc. occurs then the damage will be repaired, at the Contractor's expense, as directed by the Contract Administrator when authorized by the county or WSDOT.

SECTION 2 – MAINTENANCE

2-1 GENERAL ROAD MAINTENANCE

Contractor shall maintain all roads used under this contract in accordance with the FOREST ACCESS ROAD MAINTENANCE SPECIFICATIONS for the entire term of this contract. Maintenance is required even during periods of inactivity.

2-2 ROAD MAINTENANCE – CONTRACTOR MAINTENANCE

Contractor shall perform maintenance on roads listed in Contract Clause C-050.1 CONTRACTOR ROAD MAINTENANCE AND REPAIR in accordance with FOREST ACCESS ROAD MAINTENANCE SPECIFICATIONS.

2-3 ROAD MAINTENANCE – DESIGNATED MAINTAINER

Contractor may be required to perform maintenance on roads listed in Contract Clause C-060.1 DESIGNATED ROAD MAINTAINER as directed by the Contract Administrator. Contractor shall maintain roads in accordance with FOREST ACCESS ROAD MAINTENANCE SPECIFICATIONS.

2-4 PASSAGE OF LIGHT VEHICLES

Contractor shall maintain road(s) in a condition that will allow the passage of light administrative vehicles.

2-5 MAINTENANCE GRADING – EXISTING ROAD

On the following road, Contractor shall use a grader to shape the existing surface before timber haul. Contractor shall accomplish all grading using a motor grader with a minimum of 175 horsepower.

| <u>Road</u> | <u>Stations</u> | <u>Requirements</u> |
|-------------|-----------------|---|
| E-3000 | 0+00 to 116+21 | Grade, rip potholes, shape to crown as shown on the TYPICAL SECTION SHEET |

SECTION 3 – CLEARING, GRUBBING, AND DISPOSAL

3-20 ORGANIC DEBRIS DEFINITION

Organic debris is defined as all components of a tree that remain as by-products after the manufacture of logs, including but not limited to tree tops, branches, limbs, needles, leaves, and stumps that are larger than one cubic foot in volume within the clearing limits.

3-21 DISPOSAL COMPLETION

Contractor shall remove organic debris from the road surface, ditchlines, and culvert inlets and outlets. Contractor shall complete all disposal of organic debris, except by burning, before rock application.

3-23 PROHIBITED DISPOSAL AREAS

Contractor shall not place organic debris in the following areas:

- Within 25 feet of a cross drain culvert.
- Within 50 feet of a live stream, or wetland.
- On road subgrades, or excavation and embankment slopes.
- On slopes greater than 45%.
- Within the operational area for cable landings where debris may shift or roll.
- On locations where brush can fall into the ditch or onto the road surface.
- Against standing timber.
- Uphill side of the road.

3-24 BURYING ORGANIC DEBRIS RESTRICTED

Contractor shall not bury organic debris unless otherwise stated in this plan.

SECTION 4 – EXCAVATION

4-28 DITCH DRAINAGE

Ditches must drain to cross-drain culverts or ditchouts.

4-35 WASTE MATERIAL DEFINITION

Waste material is defined as all dirt, rock, mud, or related material that is extraneous or unsuitable for construction material. Waste material, as used in Section 4 EXCAVATION, is not organic debris.

4-55 ROAD SHAPING

Contractor shall shape the subgrade and surface as shown on the TYPICAL SECTION SHEET. The subgrade and surface shape must ensure runoff in an even, un-concentrated manner, and must be uniform, firm, and rut-free. Contractor shall accomplish all shaping using a motor grader with a minimum of 175 horsepower.

4-63 EXISTING SURFACE COMPACTION

Contractor shall compact maintained road surfaces in accordance with the COMPACTION LIST by routing equipment over the entire width.

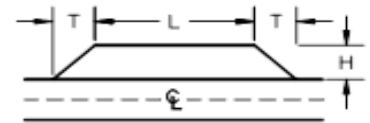
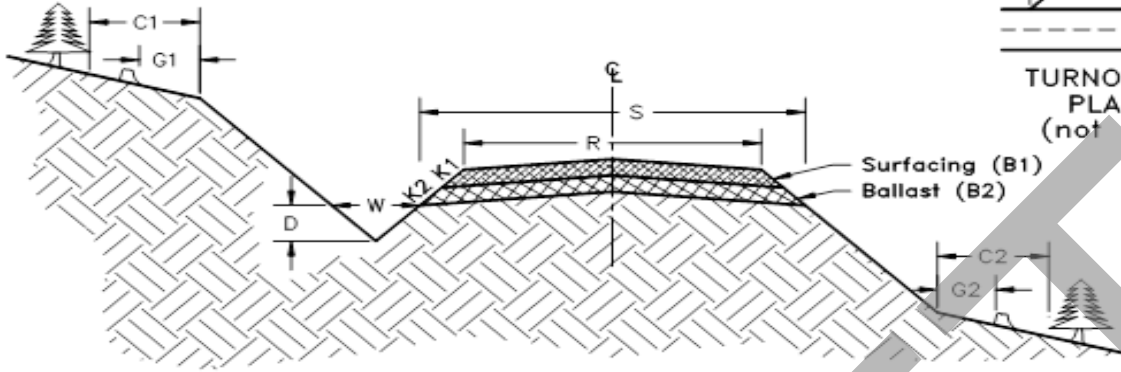
SECTION 9 – POST-HAUL ROAD WORK

9-5 POST-HAUL MAINTENANCE

Contractor shall perform post-haul maintenance in accordance with the FOREST ACCESS ROAD MAINTENANCE SPECIFICATIONS.

TYPICAL SECTION SHEET

ROAD CROSS-SECTION
(not to scale)



TURNOUT DETAIL
PLAN VIEW
(not to scale)

| Road Name | From Station | To Station | Tolerance Class | Width (ft) | | Ditch (ft) | | Crown (%) | Grubbing Limits (ft) | | Clearing Limits (ft) | |
|-----------|--------------|------------|-----------------|------------|--------|------------|---------|-----------|----------------------|----|----------------------|----|
| | | | | Subgrade S | Road R | Width W | Depth D | | G1 | G2 | C1 | C2 |
| E-3000 | 0+00 | 116+21 | A | - | 14 | 3 | 1 | 4 | - | - | - | - |

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COMPACTION LIST

| | | | | <u>Maximum</u> | | | | <u>Maximum</u> | <u>Maximum</u> |
|-------------|----------------|----------------|------------------|-----------------|-----------------------|------------------|------------------|------------------|-------------------|
| | | | | <u>Depth</u> | | <u>Equipment</u> | <u>Minimum</u> | <u>Operating</u> | <u>Amount of</u> |
| <u>Road</u> | <u>From</u> | <u>To</u> | | <u>Per Lift</u> | <u>Equipment</u> | <u>Weight</u> | <u>Number</u> | <u>Speed</u> | <u>Deflection</u> |
| <u>Name</u> | <u>Station</u> | <u>Station</u> | <u>Type</u> | <u>(inches)</u> | <u>Type</u> | <u>(pounds)</u> | <u>of Passes</u> | <u>(MPH)</u> | <u>(inches)</u> |
| All | - | - | Existing Surface | - | Vibratory Smooth Drum | 20000 | 5 | 3 | 1 |
| All | - | - | Subgrade | - | Vibratory Smooth Drum | 20000 | 4 | 3 | 1 |
| All | - | - | Embankment | 12 | Excavation | 30000 | 4 | 3 | 2 |
| All | - | - | Fill | 24 | Excavation | 30000 | 4 | 3 | 2 |
| All | - | - | Rock | - | Vibratory Smooth Drum | 20000 | 5 | 3 | 1 |
| All | - | - | Waste Area | 24 | Excavation | 30000 | - | - | 4 |

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FOREST ACCESS ROAD MAINTENANCE SPECIFICATIONS

Page 1 of 2

Cuts and Fills

- Maintain slope lines to a stable gradient compatible with the construction materials. Remove slides from ditches and the roadway. Repair fill-failures with selected material or material approved by the Contract Administrator. Remove overhanging material from the top of cut slopes.
- Waste material from slides or other sources shall be placed and compacted in stable locations identified in the road plan or approved by the Contract Administrator, so that sediment will not deliver to any streams or wetlands.
- Slide material and debris shall not be mixed into the road surface materials, unless approved by the Contract Administrator.

Surface

- Grade and shape the road surface, turnouts, and shoulders to the original shape on the TYPICAL SECTION SHEET to provide a smooth, rut-free traveled surface and maintain surface water runoff in an even, unconcentrated manner.
- Blading shall not undercut the backslope or cut into geotextile fabric on the road.
- If required by the Contract Administrator, water shall be applied as necessary to control dust and retain fine surface rock.
- Surface material shall not be bladed off the roadway. Replace surface material when lost or worn away, or as directed by the Contract Administrator.
- Remove shoulder berms, created by grading, to facilitate drainage, except as marked or directed by the Contract Administrator.
- For roads with geotextile fabric: spread surface aggregate to fill in soft spots and wheel ruts (barrel spread) to prevent damage to the geotextile fabric.

Drainage

- Prevent silt bearing road surface and ditch runoff from delivering sediment to any streams or wetlands.
- Maintain rolling dips and drivable waterbars as needed to keep them functioning as intended.
- Maintain headwalls to the road shoulder level with material that will resist erosion.
- Maintain energy dissipaters at culvert outlets with non-erodible material or rock.
- Keep ditches, culverts, and other drainage structures clear of obstructions and functioning as intended.
- Inspect and clean culverts at least monthly, with additional inspections during storms and periods of high runoff. This shall be done even during periods of inactivity.

FOREST ACCESS ROAD MAINTENANCE SPECIFICATIONS

Page 2 of 2

Preventative Maintenance

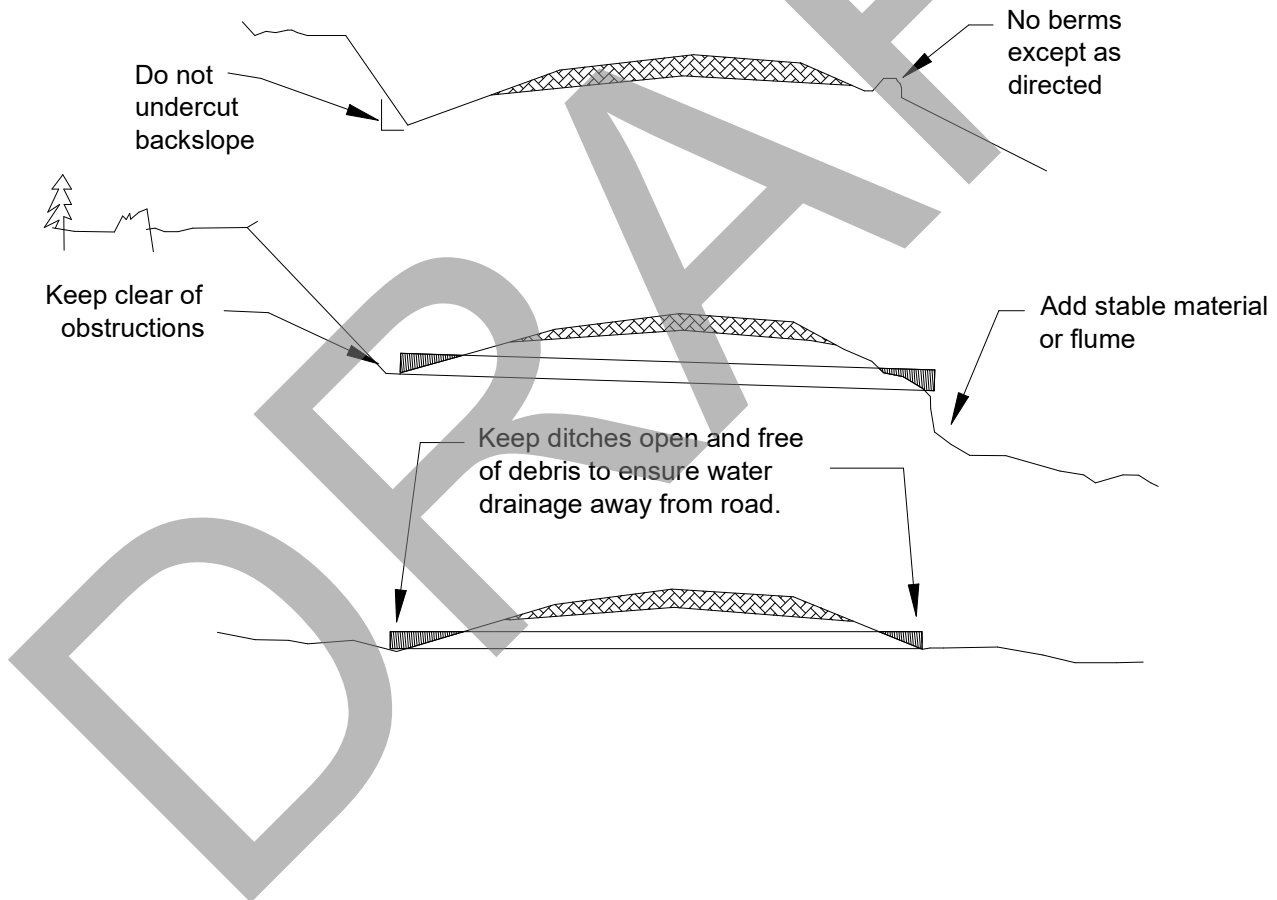
- Perform preventative maintenance work to safeguard against storm damage, such as blading to ensure correct runoff, ditch and culvert cleaning, and waterbar maintenance.

Termination of Use or End of Season

- At the conclusion of logging operations, ensure all conditions of these specifications have been met.

Debris

- Remove fallen timber, limbs, and stumps from the slopes, roadway, ditchlines, and culvert inlets.



SUMMARY Road Development Estimate
 REGION Pacific Cascade
 DISTRICT St Helens

SALE/PROJECT NAME Lickety Split Sorts

AGREEMENT NO. 30-106665

| ROAD NAME | E-3000 | | |
|---------------------------------------|--------------|----------------|-------------|
| ROAD STANDARD | Construction | Reconstruction | Maintenance |
| NUMBER OF STATIONS | 0.00 | 0.00 | 116.21 |
| CLEARING & GRUBBING | \$ - | \$ - | \$ - |
| EXCAVATION AND FILL | \$ - | \$ - | \$ - |
| MISC. MAINTENANCE | \$ - | \$ - | \$ 3,768 |
| ROAD ROCK | Optional | \$ - | \$ - |
| | Required | \$ - | \$ - |
| | Total | \$ - | \$ - |
| ROCK STOCKPILE PROD | \$ - | \$ - | \$ - |
| CULVERTS AND FLUMES | \$ - | \$ - | \$ - |
| STRUCTURES | \$ - | \$ - | \$ - |
| MOBILIZATION | \$ - | \$ - | \$ 1,500 |
| TOTAL COSTS | \$ - | \$ - | \$ 5,268 |
| COST PER STATION | \$ - | \$ - | \$ 45 |
| ROAD DEACTIVATION & ABANDONMENT COSTS | \$ - | \$ - | \$ - |

| | |
|--|----------------|
| TOTAL (All Roads) | \$5,268 |
| TOTAL (Minus Optional Rock) | \$5,268 |
| SALE VOLUME MBF | 2375 |
| TOTAL \$/MBF \$ | 2.22 |
| TOTAL \$/MBF (Minus Optional Rock) \$ | 2.22 |

ESTIMATED BY Chace Johanson

MOBILIZATION

SALE/PROJECT NAME Lickety Split Sorts
CONTRACT # 30-106665

PRE-HAUL/CONSTRUCTION EQUIPMENT

MOBILIZATION

| | | | | |
|--------|----------|--------|---|----------|
| Grader | \$ 1,000 | each x | 1 | \$ 1,000 |
| Roller | \$ 500 | each x | 1 | \$ 500 |

MOBILIZATION TOTAL \$ 1,500

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SUMMARY OF ROAD

SALE/PROJECT NAME Lickety Split Sorts
CONTRACT # 30-106665
ROAD NAME E-3000

Required pre-haul maintenance (stations) 116+21

REQUIRED PRE-HAUL MAINTENANCE

MISC. MAINTENANCE

Maintenance grading
Maintenance rolling

| | | |
|------------------------|--------|----------|
| \$ 18.03 per station x | 116.21 | \$ 2,095 |
| \$ 14.39 per station x | 116.21 | \$ 1,673 |

TOTAL ROAD COST \$ 3,768

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WASHINGTON STATE DEPARTMENT OF NATURAL RESOURCES

FOREST EXCISE TAX ROAD SUMMARY SHEET

Region:

Timber Sale Name:

Application Number:

EXCISE TAX APPLICABLE ACTIVITIES

Construction: linear feet
Road to be constructed (optional and required) but not abandoned

Reconstruction: linear feet
Road to be reconstructed (optional and required) but not abandoned

Abandonment: linear feet
Abandonment of existing roads not reconstructed under the contract

Decommission: linear feet
Road to be made undriveable but not officially abandoned.

Pre-Haul Maintenance: linear feet
Existing road to receive maintenance work (optional and required) prior to haul

EXCISE TAX EXEMPT ACTIVITIES

Temporary Construction: linear feet
Roads to be constructed (optional and required) and then abandoned

Temporary Reconstruction: linear feet
Roads to be reconstructed (optional and required) and then abandoned

All parties must make their own assessment of the taxable or non-taxable status of any work performed under the timber sale contract. The Department of Revenue bears responsibility for determining forest road excise taxes. The Department of Natural Resources developed this form to help estimate the impact of forest excise taxes. However, the information provided may not precisely calculate the actual amount of taxes due. The Department of Revenue is available for consultation by calling 1.800.548.8829.
(Revised 9/18)